



GYC Spring 2026 Newsletter



Georgian Yacht Club – Owen Sound

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There are a number of interesting articles in this Spring 2026 newsletter.

Among the most interesting and impactful for the club and its members are the efforts to get more participation by members in club work, the dredging plans for this fall, and the investigations into the state of the basin walls at the club.

- The questions surrounding the best way to enhance the volunteer activities of members was the topic of a GYC Town Hall held on March 15th and the subject of an article inside (page 4)
- The dredging is having to be done because of the erratic, and currently lowered, water levels of Georgian Bay. A more complete discussion of this is inside (page 7).
- The research and planning on the state of the basin walls was also the topic of a GYC Town Hall, held on March 29th. An article that goes into some depth on the work of the Planning Committee and the Basin Wall Committee, and the discussion at the Town Hall, is inside at (page 8).

Reports on their activities from all the Directors are also in this newsletter, as well as info on the May 2 splash-in and other regular activities.

-Joseph Hardin, Communications



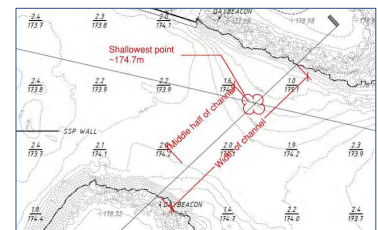
AGM Meeting

Annual General Meeting,
Wednesday
April 8, 2026, 1900H

LAUNCH 2026

- May 2 Launch & Lunch
- May 3 Cradle Moving

DREDGING September 2026



Due to Low water levels, the entrance to the GYC basin will be dredged this September, if all the permits and arrangements line up - and it looks like they will. This means the basin will be closed to entry from Georgian Bay for five days while the work is done. Exact dates will be posted as soon as they are known. For the full story, see page 6.

From the Departing Commodore

Dear Fellow Georgian Yacht Club Members,

This will be my final report as Commodore. Just two years ago, we welcomed six new board members — a 50% turnover of the entire Board. There was certainly a significant learning curve as we worked to get organized, but I am proud to say the Board has done a tremendous job over the past two years. I extend my sincere thanks to each Board member for their dedication and support, which made my role much easier.



We are currently examining our infrastructure and developing a long-term plan to ensure the Club's future growth and viability. The Planning Committee has done outstanding work mapping out a thoughtful and strategic plan for the years ahead. Jason Lehtovaara has established a clear system to monitor and plan basin repairs over the next 15 to 20 years. He has also worked diligently toward arranging dredging, which we are hopeful will take place in the fall of 2026.

John Griffith and Ian Boddy have prepared and submitted our dredging application to the six required agencies to obtain the necessary permits. The application, including its attachments, exceeds 30 pages. I extend special thanks to John for his exceptional effort and commitment to the Club.

We have also established a Member Accountability Committee to develop a system for tracking member participation. This program will be introduced in 2026 as a trial year, allowing Directors to track member hours and refine the process. The system is planned to come fully into effect in 2027.

In addition, we are developing guidelines regarding the seaworthiness of vessels within the Club and implementing procedures to ensure that all boats at Georgian Yacht Club are safe and fit for use.

Over the past two years, we were pleased to reintroduce the Commodore's Away Party. Both weekends at White

Cloud Island were well attended and thoroughly enjoyed by those who participated.

I am very confident that the new Board, led by your incoming Commodore, Chris Haslam, will continue advancing these important initiatives for the benefit of our Club and its membership. It has been an honour to serve as your Commodore. Respectfully, *-Mike Forcier, Commodore*

From our New Commodore

At this time I'd like to take a moment to introduce myself to those club members who don't know who I am. I joined the GYC about 11 years ago, with my wife Christine and my 2 boys Joshua and Benjamin. Some of you will know Joshua as he did our gas dock and lawn maintenance in 2020 and 2021. Since joining the GYC we/our boat has been located in the south basin and have enjoyed many summers circumnavigating Georgian Bay. I joined the board 8 years ago and was Property and Grounds director, followed by 2 years of Gas dock and Safety, and another 2 years back as Property and Grounds. The last 2 years I have served as Secretary/Vice commodore, while still assisting with the dock replacement project.

2026 and 2027 will be busy years around the club, with dredging and revetting/wall maintenance as large ticket items at the top of the list. I would like to encourage everyone to come out and volunteer as this is a member owned/run club and we continue to enjoy the property and the bay because of our members. Looking forward to seeing everyone at our social events and on the water.

Have a great boating season,

*-Chris Haslam,
Commodore 2026*



Launch May 2, 2026

PLEASE REVIEW THE LAUNCH AND HAULOUT GUIDELINES ON PAGE 21

It is almost time again to lift the boats into the water. Lift in this year, as always, is the first Saturday in May. This year is Saturday May 2nd starting at 8 am. If you are not able to attend the launch of your boat, you must notify me no later than Friday May 1st.

The large crane will be starting at the Northeast corner and heading towards the gas dock. The plan which of course is always subject to change is to finish at the gas dock about noon. Afternoon with the large crane will be at the old gas dock starting with the 4 just inside the South gate then Chambros before continuing East.

Small crane will be starting in front of the clubhouse headed towards the mast crane then heading around the West basin towards the South gate. Lunch break for the small crane is usually about the bottom of the gravel hill (NW corner of West Basin).

Cradle moving will be the day after the lift in, Sunday May 3rd, starting at 830am. Nick Sewell will be helping again with his boom truck. Plan to haul cradles with trucks from around the club up the gravel hill, unhook, then the truck will continue around the boom truck and out the manual gate to fetch another cradle while that cradle is lifted into the rack.

If you have any thoughts about selling your boat, please let us know so that we can make sure your cradle is stored more accessibly.

Call or text 519-387-6465

-Kurtis Mink, Launch and Haulout Director



NEW Wi-Fi replaces the old wi-fi option.

[Learn more on page 12](#)



Mark Your Calendars Important Dates for 2026

May 2	Launch Day
May 2	Cradle Moving
May 9	Spring Work Day
May 30	New Member Day
June 20	Sailpast
June 21	Sailpast Race
July 29	Georgian Bay Regatta begins
Aug 15	Commodore Away
Aug 28	Salmon Spectacular begins
Sept 18-19	GYC Fishing Derby
Sept 24	Commodore @ Home
Oct 17	Cradle moving
Oct 23-24	Haulout



40th Anniversary Annual Regatta

July 29 - August 2, 20

[See article on page 17](#)

Town Hall Meeting March 15, 2026

On a Sunday afternoon approximately half of our members gathered to find out what were the latest thoughts regarding a plan by the Board to promote the sharing of the work involved in maintaining our D.I.Y. club. The old gripe about 20 % of members doing all the work and the frustrations that occur from that amongst the Board and membership continues. In an effort to rectify that situation and equalize work amongst all members, the Board organized a group to look into how other clubs handled this problem and find solutions

Discussions by some members of this group with other clubs concluded that the most popular method to encourage equal work was a per hour of work fee charged to members payable with the following seasons membership fees. That fee was eliminated for members who provided a minimum required amount of labour during the season and pro-rated for those who did not. All hours of labour were dutifully recorded, and all clubs were generally happy with the results.

Questions at the town hall were mostly positive. There was some concern that the program was going to increase membership fees substantially by charging a suggested 10 hours of labour at \$50. per hour to be worked off and records kept by the Director for whom the work was being done. Ten hours would result in zero extra charges to next year's fees for that member. There were grumblings in the audience after it was suggested that perhaps two years of non-work could result in cancellation of membership. Threats are never perceived as a positive.

There was much discussion as to how all of this record keeping was to be accomplished and how the work was going to be created and divided amongst the committee members. Some thought given as to how imbalanced the present committees are in terms of numbers and how those numbers needed to be balanced to make work efforts equitable. Some suggestions that members may have special skills and they should perhaps be in other groups.

The Board plans to augment this proposal next fall and in the meantime, they plan for committees to start recording labour hours to get a better feel of how this might work and what changes are needed for the idea to succeed. The Board is only authorized to raise fees by 10% so that means possibly a vote by the membership at

the annual general meeting next year. There were lots of thoughts and discussions after the Town Hall as members thought more about the proposal. One thing that is certain, we ALL need to help keep this club functioning well into the future with the amazing benefits that we enjoy.

-Chris Collard

Town Hall – March 29, 2026



The GYC Town Hall held on March 29, the second Town Hall in the last few weeks, was to inform the membership about dredging plans and planning for the monitoring, repair and reinforcement of the basin walls. Over 50 people attended the meeting, with the room full, and coffee and cookies were available. The articles in this newsletter on these topics give the background on these issues, but the discussion at the Town Hall raised and answered lots of questions about just what was going to be done.

Jason Lehtovaara presented the history of the work the Planning Committee and its subgroups have done in 2025 and 2026. Starting with a focus on planning for the short, medium and long term. Evaluating the risks and the impact of any work at GYC is part of the mandate of the Planning Committee. These are then reviewed with the Board with a view of the next 50 years. The dredging needs to be done now, as some boats simply can't get out if it is too shallow. The basin wall work is different. Repairs could be done now on the most serious areas, monitoring done on all the walls, and plans

put in place for longer term efforts that are needed, with an eye on both making sure the club's facilities remain robust and that costs can be as low as possible and spread out over a reasonable time frame. Planning for the longer term was presented as key here.

Jason emphasized the need for volunteer support for as much of the work as possible and encouraged everyone to contact the team project leaders about any support they could provide. From shovel and wheelbarrow operators to welders to heavier equipment operators of various types will be needed.

Questions regarding the dredging covered a wide array of concerns. Members wanted to ensure that the timing would not interfere with club events such as fishing derbies. There were inquiries about the installation of temporary channel markers to guide boaters in finding the safest and deepest path throughout the season. Additionally, the importance of keeping neighbors informed about the ongoing work was highlighted. Attendees also raised issues about managing the sediment excavated from the entrance, with further questions about the expected post-dredging depth and the frequency at which dredging might need to occur in the future.

If the lake levels were to go up, then the dredging wouldn't be needed. A few years ago, in 2013-2014, the water levels were low, and the club was all ready to dredge, paperwork all done, but then the water levels rose. This time we've not been as fortunate. With climate change, variability in precipitation, winter ice coverage, and temperatures increasing, and predicting the future of water levels is becoming more difficult. In addition, silt is deposited regularly in our entryway due to the configuration of the breakwalls and neighbouring

structures, so dredging is made more frequent. Long term plans, including significant expansion of the club area, were raised by the members.

It was communicated that we have contaminated sediment. The cost of the dredging is in the \$100,000 range, but could vary significantly, up and down, depending on what the level of contaminants is after the



sediment dries out and it is turned to allow evaporation of the contaminating hydrocarbons it contains. Trucking the lower-level contaminated residue to Miller Lake, where there is a site that can accept low level contaminants, is much cheaper than trucking higher level residue further away to a site that can accept it. Testing of the material from the entryway will be done as it dries to see if it has become reduced in contaminants below the threshold for different disposal options.

When it came to the basin wall infrastructure, the components that are degrading were categorized as concrete panel cracking, H King Pile undersized, integrity of tie backs and erosion control. The priority areas around the mast crane and the west basin were discussed and the floor was turned over to Ralph Suke, who walked the audience through his proposals for mitigating the problems with the most seriously affected areas of the walls. (See basin wall article). The changes and improvements he outlined would make the walls

viable for many years to come and could progressively be applied to all the different segments of the walls.

Information from members in the audience like Ed Griffith and Donald Sutherland contributed to the understanding of what has been done in the past, like basin dredging in 2000, and what details of the existing tiebars and anchors underground look like. One of the issues discussed was how to determine how fast the walls are moving, and Ralph suggested the club invest in an inexpensive inclinometer to measure the drift. If we see further drift, we will have to intervene soon but if it remains stable, we will be able to defer significant repairs or replacements. This will give us time to prepare to fund the projects to ensure the club remains viable for the next 50 years.



Another issue focused on mitigation and prevention, what we need to do to relieve the walls of as much stress as we can to prolong their life. Reminders were made to dock boats with the bow in, so prop wash does not cause undermining of the walls. It was suggested that parking near areas that were weakened be avoided. Ralph put in a plug for AI here, pointing out it's getting easy to get usable results from using it, with the example of determining the varying loads on the wall from weights placed 5 feet, 7 feet or 10 feet back from the wall. By the time you get 7 feet back from the wall, the load on the wall, the forces pushing it out, have largely been reduced to very low levels, and by 10 feet they're largely gone. This could affect, for instance, how we see the use of the storage area in the west basin, where we put boats in the winter. The effects of the cranes that lift boats in and out was also brought up, with a view to minimizing any damage their outriggers close to the wall could cause during launch and lift out.

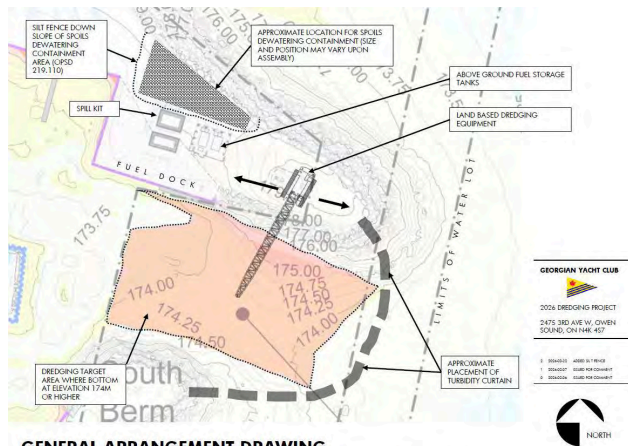
At the conclusion the members thanked Jason for the presentation and the Planning Committee members for all their work with a round of applause, and the meeting broke up into a number of small groups as everyone discussed further the topics brought up.

*-Joseph Hardin,
Communications*



Dredging Plans

This September, the entryway to the Sound from the GYC basins will be dredged to increase the depth and make sure boats with deeper draft can easily transit the entryway. Bay and Lake levels have been low the last couple years and predictions indicate a further decrease in 2026, with a potential drop of 7–8 inches. So, before the dredging takes place, that is, over most of this sailing season, it is important for vessels that draft more than 5 feet to carefully assess their operating this season.

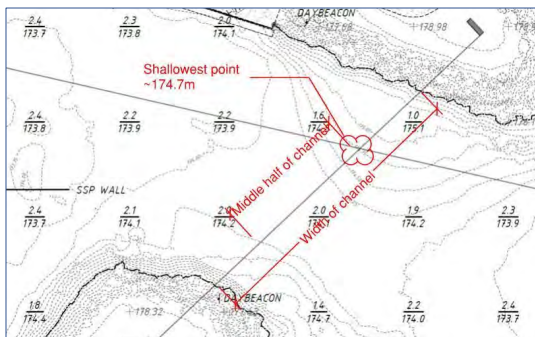


GENERAL ARRANGEMENT DRAWING
DO NOT SCALE. ARRANGEMENT IS REPRESENTATIVE. EXACT LOCATIONS AND EQUIPMENT MAY DIFFER

A larger version of this image is at the end of the newsletter.

Credit – John Griffith

Markers, buoys, will be installed to ensure safe passage through the club entrance, marking the best channel for getting into and out of the club this year, and the range lights will probably be turned off to prevent false guidance if used as navigational aids. Prior to launching, boaters with deep drafts should measure basin depths and evaluate associated risks. Around 18 of the club’s sailboats have a draft exceeding 5-5.5 feet and will be particularly affected. Check your charts, and the graphic showing the hump just before the gas dock in the entryway.



A larger version of this image is at the end of the newsletter

The dredging will require closing the entrance to the GYC basins for 5 days while the work proceeds, and the timing will be set to not interfere with the Fishing Derbies that take place late in the season.

Investigating the task of deepening the entryway and coming up with detailed plans and procedures for the dredging that needs to be done has been part of the Planning Committee’s efforts over the last months. The Dredging Committee has been composed of Ian Boddy, John Griffith, Tom England, Kurtis Mink, H. Vandestadt, and B Toby, with Jason Lehtovaara as the lead. Planning has involved permit applications, soil testing, evaluation of costs, determining contractor availability, and figuring out the best way to deal with the lake sediments that will be dredged up. Permits were needed from a number of bodies, including Department of Fisheries and Ocean, Grey Sauble Conservation Authority and the Saugeen and Ojibwa Nations.

The lake sediment testing that has been done, by GEI soil consultants, has revealed that two contaminants are there. These are common hydrocarbon contaminants often found in areas with high boat traffic, products of incomplete combustion and fuel spills. Their presence necessitates handling the dredged sediment carefully. It estimated that around 250 cubic meters, that’s in the neighborhood of 300 tons of sediment will need to be dredged. An area behind the gas dock (see graphic) will be used to store the sediment as it is dredged, and the drying sediment will be turned over on occasion to help it aerate and to help the volatile hydrocarbon contaminants evaporate, lowering the level of contamination of the sediment. If the drying, turning and aerating are sufficiently successful, then the sediment will be easier and cheaper to dispose of. It will be tested as this operation proceeds to see how it is going.

A good bit of this work will be done by GYC volunteers, so if you can help get in touch with Jason Lehtovaara, the dredging committee lead, or any of the dredging committee members.

Future considerations for work on the GYC entrance have also been discussed. The current configuration of breakwalls, ours and our neighbour’s, contribute to silt being deposited in the existing entryway, requiring regular dredging with all its costs to the club. It’s possible to design new structures that would minimize or remove this problem. This will be a topic for future discussion.

The dredging plans and surrounding issues were one of the topics of the GYC Town Hall held on March 29. See the article on the Town Meeting for further info.

-Joseph Hardin, Communications

Join us at SAILPAST 2026
June 20, 2026

Photos from past Sailpast Events

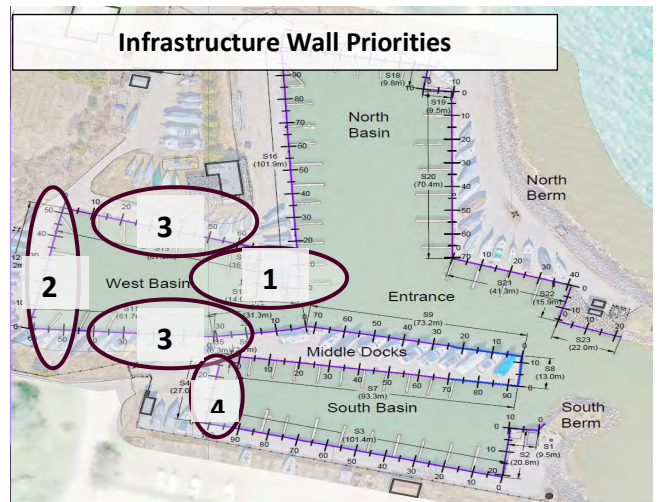


Basin Wall Report



The GYC has been looking at long term planning for the club, and part of this has been evaluating the state of the basin walls. Along with the entryway, the basins where all our boats reside is a key element of the GYC. As the commissioned report on the state of the club's basin walls by the ShorePlan consulting company has detailed, there are some areas of the walls that are in need of maintenance or repair. The full report is at <https://www.georgianyachtclub.com/shoreplan-report-on-state-of-gyc-marina/>

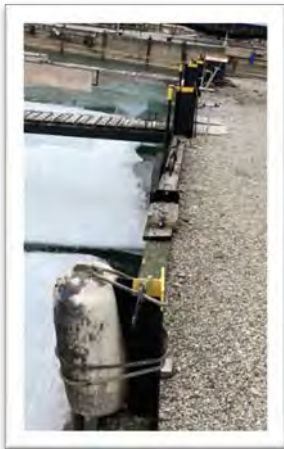
Starting from there a subgroup of the Planning Committee, the Infrastructure Group, composed of D Bishop, R Wright, T Potter, and Jason Lehtovaara, and led by Ralph Suke, who in a previous life was a civil engineer, has been investigating ways to mitigate the risk posed by any deterioration in the basin wall structures, and developing ways to extend the life of the basin walls for as long as possible. Many of these walls were put in 40-50 years ago, and it's natural to expect some work is needed to keep them in shape. Over the years club members have on various occasions done repair and



A larger version of this image is at the end of the newsletter

viability of the structures well into the future, over coming decades.

The walls in most of the basins are composed of rebar reinforced concrete slabs inserted into the slots of H shaped steel beams (called I beams) that are themselves driven 10 feet into the basin floor. These beams are then held in place, that is held back from leaning out under the pressure of the ground behind them, by steel rods, tie-backs, that go a number of feet back into the ground and are themselves attached to anchors dug into the ground.



As can be seen from pictures, or just walking around and visually inspecting the walls, there are some places where these concrete panels are leaning out. Further inspection shows that there has been some erosion under some of the panels, leaving a gap between the bottom of the panel and the basin floor.

The Infrastructure Group has come up with plans to extend the life of the walls for many years. As Ralph Suke detailed in the Town Hall Meeting, the work that needs to be done involves digging out behind the existing walls, placing clear stone behind them, and installing new tiebacks 2 feet down anchored to an I beam buried 15-20 feet back, in the process inspecting existing tiebacks and repairing any that need it. In addition, clear stone would be placed on the basin side of the concrete slabs at their foot to help reduce any erosion there. Further reinforcement would consist of a steel bar placed on the basin side face of the concrete slabs running between the I-beams to give further support to the concrete slabs.

There are places that need this work more than others, and they will be dealt with first. The west side of the mast crane area and the west wall of the west basin are two of the high priority areas. (See graphic) Plans are currently being finalized and the work could proceed this summer. If so, this will be another place that significant volunteer help will be needed. Stay tuned.

-Joseph Hardin, Communications

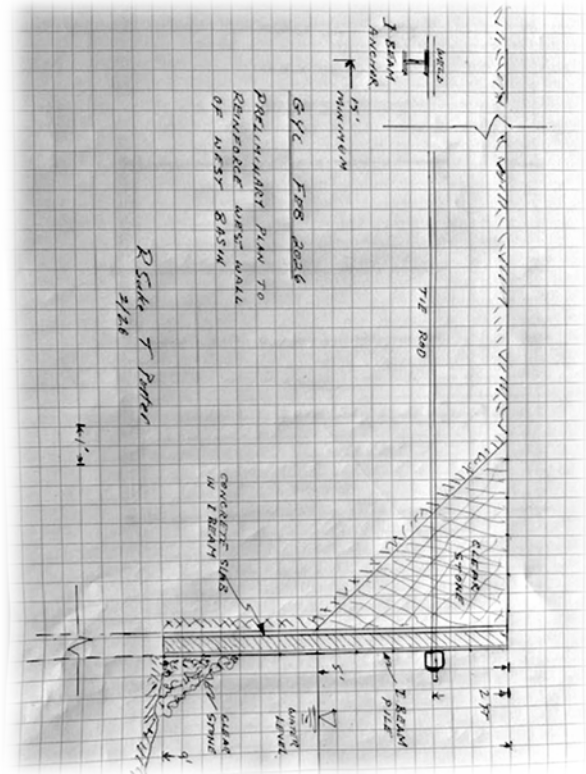
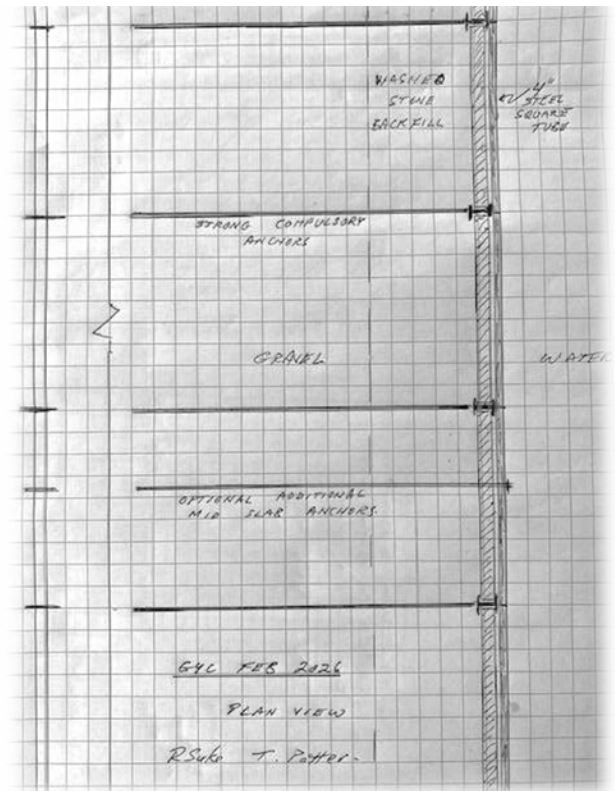


Image credits - Ralph Suke

Larger versions of these images are at the end of the newsletter



Planning Committee

As we anticipate the arrival of spring and the upcoming boating season at GYC, it is a fitting moment to reflect on our progress and future plans.

This past year has been productive, with multiple inspections conducted throughout the property. A reputable Marine Engineering firm evaluated the dock walls and docks, documented their condition to project remaining service life, and identified necessary repairs. An Information Session will be scheduled before launch to review the findings in the following areas;

- Bathymetric (water) and topographic (land) surveys were completed to guide recommendations regarding dredging requirements and basin planning.
- The team assessed shore structures and docks, presenting maintenance and repair options addressing various timeframes: short-term, 7–10 years, and 15-30 years.

GYC continues to build upon a legacy of volunteerism and quality craftsmanship. This was underscored by the recent ShorePlan Assessment of our property and basin wall infrastructure. The evaluation found no immediate safety concerns; however, challenges remain regarding basin depths and several underwater wall segments, which are in suboptimal condition. Notable issues include cracks in linked concrete panels and the use of lighter-gauge King Pile (H-beam), which lacks sufficient design margin for extended future use. The walls are exhibiting a pronounced lean, which is being exacerbated by reduced water levels.

We are currently formulating strategies to monitor and strengthen the perimeter of the basin, placing particular focus on the West Basin. These initiatives will enable us to assess the appropriate schedule for any subsequent repairs. Success will rely strongly on collaborative teamwork.

The focus for this summer will be planning and preparing for dredging the club entrance. The Planning Committee is prioritizing this work to improve navigation margins, particularly given current low

water levels on the Great Lakes. The bathymetric survey has precisely identified where dredging is required, and preparations are underway for permit applications and cost assessments. As the permitting window for these activities runs from July 15 to September 30, 2026, dredging operations are scheduled for September 2026.

Additionally, the GYC Board of Directors has approved preparations for a comprehensive long-term plan, covering the next 10–15 years, to forecast capital projects and ongoing maintenance of walls, docks, grounds, and properties. This proactive approach aims to safeguard the club's future by strategically monitoring and strengthening infrastructure, extending lifespan, and coordinating improvements to support the continued viability of GYC.

Wishing everyone an enjoyable and successful 2026 season.

-Jason Lehtovaara, Planning Committee Director

Property and Building Report

The South Gate was replaced. In 2027 the north gate will be replaced.

We will be sending out a notice to all the people on property and building list just after the AGM because this spring we are refurbishing four docks in the west basin and one in the north

We are installing some piles for docks and adjusting piles that are leaning bad as well.

Happy boating everyone!

- Trevor Grassley, Property and Building Director



GO FISHING!

**37th Annual Owen Sound
Salmon Spectacular**

August 21 to September 6, 2026

GYC Fishing Derby
September 18-19

Mooring Report

Hello fellow Members of the Georgian Yacht Club!

The Mooring Committee only had 2 vacancies to assign docks for this winter. A committee meeting was held in March to assign vacancies and resolve a small number of requests from the Dock Request List to ensure our basins are full of boats this coming season.

I invite all Members to look up the mooring policies and procedures on the Club's website and get familiar with how they apply to you. Over the winter did you come down with a case of two-foot-itis? I would like to remind any members that are in the market to buy a larger boat, that right now, we do not have room to accommodate additional larger boats in the Club. If you're thinking about getting a bigger boat, please take a look at the Dock Request List posted on the Mooring Committee page on the website where you will find a healthy backlog of Members looking for bigger slips. Many Members have been waiting for years!

Bottom line is that if you get a new boat and it does not fit on your current dock then you must find accommodations elsewhere while you wait.

I would also like to recognize the late Jamie Harrison for his contributions to the Mooring Committee at the Georgian Yacht Club – Jamie served on the Mooring Committee for over 24 years and 14 of those years he served as director. Jamie was always evaluating how best to distribute out dock space in the most fair and measured way. Fair winds old friend!
Respectfully Submitted,

-John Griffith, Mooring Director



Transient Reservation List

Are you planning on fishing in the derbies this autumn with a second boat?

Are you transitioning to a new boat and need temporary dock space for your boat that is for sale?

Need a dock to tie a large tender and dinghy to while you prepare for your boat trip?

Need a dock for a weekend here-or-there to sea-trial your project boat?

Space for second boats is limited and not guaranteed. Get your name on the reservation list today by emailing mooring@georgianyachtclub.com with the size of boat and planned duration of your stay.

Spots will be awarded based on request date so get your request in TODAY!

*Reservation list opens January 1, every year.

**2026 dredging work may impact availability.

***Preference may be given to requestors that make arrangements with Members with vacant docks.

****Proof of insurance required for all boats on GYC property.

Thanks, GYC Mooring Committee

GYC Security Committee

GYC Security System Reminder:

Security monitoring ends day of launch but cameras remain active 24/7. After this date, you no longer have to disarm the system between 11 pm and 6 AM in the morning. It gets turned on again at haul out.

-Steve Furness, GYC Security Committee

Gas Dock & Safety

We have a small repair on our Diesel Tank waiting for warmer weather. In discussions with our fuel supplier, we saw no benefit in topping up the tanks last fall and letting the fuel "age." Both diesel and the gas will be a fresh mix when the Fuel Dock opens in May.

There is no change in the days and hours available, but I proposed a similar programme to the Board that may add to the availability of fuelling times. The extra hours we Opened for the Salmon Spectacular was well received.

I cannot emphasize enough, that the use of marine grade toilet paper is the only TP that you should be using on your vessels, if you want to use the GYC pump out facility. As a common courtesy to the next user of our pump out system, putting solids in black water tanks, namely feminine hygiene products will jam our system and shut down the system indefinitely. Our pumps are "gear" pumps designed for fluids only and will NOT move non-compressible items.

Finally, as a Volunteer based organization, we need everyone to go online if you haven't done so already, and take the FREE "Oil Spill Prevention and Response for Marina Staff" at <https://boatus.org/spill-prevention>. As a Club member and boat owner, you never know when you will be called upon initiate or assist in stopping or reducing an Environmental incident.

I am looking forward to reporting out dates and timings in the near future. If you have any concerns, please contact a member of the Gas Dock & Safety Committee.

-Murray Arnold, Gas Dock & Safety Director



FREE CLASS: Oil Spill Prevention and Response for Marina Staff and Boaters

<https://boatus.org/spill-prevention>

New- Improved Wi-Fi



As part of the wi-fi upgrade we've been working on, there is now a new wi-fi network available to you at the club called **GYCcrew1**. (*YachtC and yachtC will no longer be available*).

The password for **GYCcrew1** is listed at the bottom of your Profile page on the GYC website. We put it there instead of including it here to reduce the chances of non-members getting a hold of it.

To see the password on your Profile page, use this link: <https://www.georgianyachtclub.com/my-gyc-profile-details/>

OR go to <http://georgianyachtclub.com>

- Login
- Click on Members/Associates
- Click on My GYC
- Click on My Profile
- Scroll to the bottom of the profile page to see the new password.

Social Committee

Our Social Committee has had a very quiet winter, ever since hosting a very fun and successful Christmas Party on Nov. 29th (photos attached). It was truly a blast for all in attendance, with great pot-luck food and wonderful (silly) party games provided by our members, with the only downside being that the attendance numbers were not all that encouraging (but BIG! Shout-Out and Thank You to all those who did show up and Party-Hearty with us).





We have in fact, only held one meeting so far this winter, on Feb.4th. Although the attendance was somewhat sparse in numbers, a large number did send regrets (as they were unable to attend for various reasons, including vacations, etc.). Of those who could be there, there was none the less a very productive and encouraging meeting

The discussion highlighted the challenges we have encountered with winter activities at the Club, with members questioning the viability of winter events, given the steadily declining interest over the past few years, the difficulty in coming up with an event that would attract participation and issues regarding accessibility to the Club property, given the winter snow conditions these past two winters. After considering a number of possible winter club events and operational matters, we ultimately agreed to cancel the mid-winter social event this winter, primarily due to low attendance expectations, due to lack of interest and accessibility concerns.

We did discuss and decided to attempt to again engage our members in another survey, in an attempt to both gauge the feasibility of winter events and look at ways to enhance and improve our boating season events. The younger families and children in club activities, with suggestions that we hold family-friendly events, together with our usual event, but starting earlier in the day, to accommodate young parents and their children. In particular, regarding incorporating children's activities into club events. In particular, we looked at Commodore at Home; looking at starting family / children's events earlier in the



afternoon, before the dinner. We discussed setting up a dedicated kids' area, with games and crafts and possibly finding a Club member or family friend, with experience working with children to organize activities.

Our committee also reflected on the fact that our last survey clearly indicated that the greatest event popularity centered around Sail Past and Commodore at Home, borne out by the fact that they are by far the best attended. We also noted the survey also indicated the next strongest expressions of interest were for events featuring boating education events and courses or hosting boating guest speakers – which is still a work in progress.

In terms of enhancing our already most popular events, we agreed to bring back the tradition of putting physical, printed, Sail Pass Flyers and Membership Lists on every boat, in advance of Sail Past, plus sending the same information out to all members by email.

We also noted that the last survey indicated that 90% of members preferred to receive communication about Club Events via Email – which brought to mind the current initiative of have Member's partners added to the email mailing lists (because all too many of our "male" members either never look at their emails or share club event information with their partners). This led to a discussion about the problem of communicating the availability of this option to our member's partners.

This led to us planning to improve this initiative in a couple of ways: Posting a Notice and Sign-Up sheets in conspicuous places, in and around the Club, and including a Notice of the program in the physical, printed Sail Past Flyers we intend to put on all the boats in advance of Sail Past.

That's about all from your Social Committee for now, until we Gar-Up for Lift-In and Spring Clean Up. As always, if any of our fellow Club Members have any other ideas of how we can enjoy more or better Social Events, please get

in touch with John at 519-901-2190 or jwgilbert@gmail.com. And of equal importance, if anyone is interested in joining our little band of social butterflies – please also get in touch – the more, the merrier!

Wishing you all Fair Winds and Following Seas,

-John Gilbert, Social Committee

Membership Update

Spring is just around the corner, and as we all get geared up to get back out on the water, the Membership Committee has been hard at work with some house cleaning initiatives. Sue Buehler and Paul King have been actively working on updating our data bases, emergency contacts and committee list for all members of the GYC.

If you are contacted by either, this is member wide initiative, your response is appreciated. The Membership Committee partnered with the Gas Dock & Safety Committee is working diligently to host, yet another New Member/Associate BBQ and Equipment Training Day slotted for May 30th, 2026, where we look to embrace our newest GYC members and brush up on our safety & equipment operation. Everyone is welcome to attend, details to follow.

Looking Ahead

The Membership Committee will be selling off old GYC Swag Stock (coffee mugs & the like) in early 2026 with all proceeds destined for the new White Cloud Outhouse Restoration Project. Items, prices and details coming soon, watch for updates on the website and Facebook page.

Membership By the Numbers

Currently there are 148 Members, and 36 Associates. There are 21 Members awaiting slips, 4 on the A List, and 17 on the B List (see below).

GYC A List

Name	Date
Kris VanDolder	2017-10-06
Marija Scriven	2020-12-10
Bronwyn Fryer	2022-05-03
Ryan Carson	2025-07-02

GYC B List

Name	Date
Mitchell Flewelling	2021-11-02
Mike Noble	2022-06-07
Karla Lang	2023-01-10
Matt Arthur	2023-05-02
William Langdon	2024-12-03
John Denbok	2024-12-03
Terry Macrae	2024-12-03
Adam McDonald	2025-03-05
Kelly Wilkins	2025-03-05

Ron Kirk	2025-05-08
Tim Campbell	2025-05-08
Tyler Grasley	2025-09-08
Brodie McGruer	2025-09-10
David Bishop	2025-11-04
Bart Toby	2025-11-05
Jody McEachern	2025-11-06
Erika Brainard	2025-11-07

Club History

As always, your contribution to club history is greatly appreciated, please feel free to share photos, videos, or stories on the club Facebook page. If you would like your photos or stories to be posted to the club website, please send to membership@georgianyachtclub.com !

-Sarah Fenton, Membership Director, Historian



LITTLE CURRENT CRUISERS NETWORK

VHF Channel 71
9:00 am

Reporting each morning during July and August

Marine Weather • World and Local News
Sports • Community Events
Answering questions and relaying messages between boats in the North Channel

Hosted by:
Anchor Inn Bar & Grill

in association with:
Little Current Yacht Club

History of Construction at GYC

The GYC Historian and History Committee have started collecting photos and stories describing the history of construction at the GYC. Please visit <https://www.georgianyachtclub.com/history-of-construction-at-the-gyc/> to review the collection so far. We encourage you to offer details, anecdotes, articles and photographs to help the committee complete this collection that show off the incredible contributions past members have made to GYC.



Driving piles along the southern edge of the first basins, circa 1955



Expansion of west basin, circa 1957



Sun Times photo by Doug Camp
YACHT CLUB WORK — Although the next year. Dredging is being done and walls constructed at the channel and the bank alongside the clubhouse has been removed.

Dredging and expansion in 1974 - Owen Sound Sun Times.

You can see more photos at:

<https://www.georgianyachtclub.com/history-of-construction-at-the-gyc/>

Treasurer's Report

Another boating season is quickly coming, and we are getting ready for the start of a new year at the GYC. This year we have continued to bill mooring using square feet. All full members with a mooring pay a standard common membership and maintenance/utility fee. Mooring is the third common fee to all members based on the size of their boat. The use of square feet for boat size provides a more balanced approach to mooring fees than the old calculation. Some members also pay optional fees for lift, haul and storage. Brian MacIntosh works diligently on preparing the invoices to GYC members and associates.

If you are planning any changes to your lift, haul, or storage please communicate this to Brian MacIntosh at invoicing@georgianyachtclub.com. You can also always view your invoices on the website under "my GYC" tab after you have logged into your account.

Looking forward, recent assessments have identified several areas where repairs and upgrades may be required to address aging and failing systems and ensure the continued safety, reliability, and enjoyment of our facilities for all members. Given the scope of this work, it is likely that fees will need to increase to fund these essential projects. The Board is carefully reviewing all options involved in increasing revenue streams and to

manage costs responsibly while maintaining the high standards and long-term viability of our club. We are committed to transparency throughout this process and will provide detailed information, including project scope, timelines, and financial implications. Thank you for your continued support and dedication to our club.

My email is treasurer@georgianyachtclub.com and phone number is 519-881-6638.

Best Regards

-Peter Struthers, Treasurer.

Wednesday Night Racing

Attention Sailors

It won't be long until the sailing season is underway. Hopefully you are planning to get involved in sailboat racing. It's plenty of fun and a great way to improve your sailing while getting to know fellow club members!

If you aren't sure about racing your own boat, there are other boats that need crew. Contact Paul Weitendorf or Dan Jones if you are looking for a boat to crew on or if you want to start racing your own boat.

Our fleet has adopted pursuit racing. What is pursuit racing? Rather than all starting at the same time, individual boats started at times established by their handicap ratings so that, all things being equal, all the boats should finish together. It made for some exciting races, especially at the finish line.

In addition to our regular Wednesday night races, the club hopes to re-establish some PHRF handicap races.

-Paul Weitendorf



The Great Lakes Tugboat Rendezvous

...will be back along the beautiful boardwalk of Little Current Ontario **Aug 14th and 15th!** Don't miss checking out the cool tug and their captains, along with other fun Tug Boat activities! Check the Tugfest's Facebook and Instagram pages for more information.

White Cloud

Last summer saw a lot of great use of the outstation property on White Cloud Island and lots of great GYC



presence in Kidd Bay. We even saw the Georgian Bay Regatta pass through Kidd Bay which was attended by many GYC racers.

The Commodore's Commode project to build a new outhouse on the island has raised over \$600 in empty beer and liquor bottles. We still have a ways to go. The committee is going to continue collecting bottles this summer with a plan to start construction in Spring 2027 so please deposit your empties from your boat and home at the receptacle at the GYC. Big thanks to everyone that has been depositing so far.



- John Griffith

Chair, Ad-hoc White Cloud Outstation Committee



Georgian Bay Regatta



Sails, Spirit, and Stewardship: The Enduring Legacy of the Georgian Bay Regatta

The Georgian Bay Regatta (GBR), now entering its fourth decade, remains more than just a test of nautical skill; it is a traveling festival of community, history, and a vital lifeline for the next generation of local sailors.

The GYC's participation in the past has been linked to members' names like Glen Boisvert, Mark Passmore, Dave Manners, Rob Allen, and others. Mark was the overall winner in the early 2000s. This past year, our club had what is unofficially the largest participation in recent years. Five members competed for the hardware and the opportunity to show the sailing community the calibre of skilled and experienced sailors that call the GYC home. Myself, Daniel Jones, I sit on the board of directors for the Georgian Bay Sailing Association, and I proudly represent the GYC while ensuring this event is here for years to come. I hope one day my son may dream of participating as I once did as a youngster sailing at our club.

A Course Through History. The regatta's story began in 1985, born from an ambitious vision to create a week-long regatta across the bay's unpredictable waters. In its early years, the event featured gruelling non-stop races that circumnavigated the bay, challenging crews with overnight legs into Parry Sound.

By 1987, the Georgian Bay Sailing Association (GBSA) was formed to provide a permanent home for the event. Over the decades, the format has evolved to meet the needs of participants. The daunting long-distance races of the '80s and the 7-day events of the 90s have transitioned into a more accessible four-day "port-to-port" format. This shift has allowed the GBR to become a true "cruiser-racer" hybrid, where high-performance PHRF-rated yachts share the water with family cruising boats.

The GBR is unique in its nomadic nature. Unlike stationary club regattas, the GBR route alternates annually between the East and West shores, ensuring that the economic and social benefits of the fleet are shared across the region's waterfronts.



The West Side: Recent years have seen the fleet grace the West shore, visiting the harbours of Meaford, Lion's Head, and Wiarton.

The Central: This course sees the fleet starting in Collingwood. The fleet works east into Midland Bay. There they visit the ports of Penetanguishene, Victoria Harbour and Midland.

• The East Side: In alternating seasons, the race dives into the world-famous 30,000 Islands, navigating the narrow channels of Sans Souci and anchoring in the pristine waters of Regatta Bay and Kilcourse Bay.

The event concludes in Parry Sound at Big Sound Marina. "The magic of this event is that the scenery changes every day," says one veteran skipper. "One night you're tied up at a municipal dock in a small town, and the next you're 'rafting up' with twenty other boats in a wilderness anchorage, sharing a BBQ on a granite outcrop."



Investing in the Future: Giving Back

While the trophies are the formal goal, the GBR’s most significant impact happens far from the finish line. As a not-for-profit organization, the GBSA has made youth sailing development a cornerstone of its mandate. The regatta acts as a major benefactor for local sailing schools, including Sail Georgian Bay in Meaford and the Collingwood Sailing Academy. Others include the Brakewinders sailing school in Wiarton. Through annual contributions and sponsorships, the GBR helps these schools: Maintain fleets, invest in scholarship programs and Instructor Training. Supporting the certification of local youth to become the next generation of sailing coaches.

By fostering these grassroots programs, the regatta ensures that sailing remains an accessible part of the Georgian Bay lifestyle.

Looking Ahead

As the 2026 season approaches, the GBR continues to adapt. With a renewed focus on "Safe Sport" initiatives. The organizers are committed to preserving the bay we call home. Whether it's a seasoned racer looking for a tactical challenge or a cruising family seeking adventure, the Georgian Bay Regatta proves that after 40 years, the wind is still very much in its sails.

This year’s regatta is happening July 29-August 2, making it the 40th year Anniversary for this great event!

For any aspiring racers, maybe this will provide some impetus to join their fellow club members and get GYC a little more visibility in this event!

- Dan Jones, Renegade



**40th Anniversary
Georgian Bay Regatta**



July 29th to August 2nd, 2026

Collingwood – Christian Island – Discovery Harbour – Hog Bay – Midland

Organizing Authority: Georgian Bay Sailing Association

Registration Opens May 1 and Strictly Closes July 8, 2026

See More information at:

<https://ontariosailing.ca/racing-and-officials/georgian-bay-regatta/>

Date	Race	Racecourse
Wednesday, July 29	-	Christian Island to Collingwood (feeder race)
Thursday, July 30	1	Collingwood Course Race
Thursday, July 30	2	Collingwood to Christian Island
Friday, July 31	3	Around Beckwith Island
Friday, July 31	4	Beckwith Island to Discovery Harbour
Saturday, August 1	5	Discovery Harbour to Midland
Saturday, August 1	6	Midland to Hog Bay
Sunday, August 2	7	Hog Bay to Midland

"This past year, our club had what is unofficially the largest participation in recent years"

Photos by John Griffith




Using your My GYC Account

There is a lot you can do with the GYC website. You can find news and updates on club meetings and activities that are only available to members who have logged in. Log in and see what you might be missing.

The website is our way of keeping in touch with you. Every member already has a GYC login account. Make sure you log in and ensure that your personal information on your profile page is up to date.

Login to your My GYC page at:

<https://www.georgianyachtclub.com/my-gyc/>

If you do not know your login or password information – no problem- follow the instructions on the right. 

Review your Profile Page

Once you are logged in, there is a page you should visit to verify that the information Membership and Mooring have stored is correct. In the menu pull down **Members/Associates** and then choose **My GYC** and click on the **MY PROFILE** button.

Member Editable Details: Your profile page lists your name, email address, postal address and up to three phone numbers. It also lists your spouse's name and your bio if you have written one. You can correct all of this part of your profile yourself.

Click on: "To edit your own details click here."

Mooring and Membership data includes the committees you serve on, and is maintained by the respective directors. Contact them to have the information corrected or updated.

Subscribe My Spouse: Near the end of the page there is also a form where you can subscribe your spouse so that they, too, receive club wide emails.

Wi-Fi and Gate Code: Finally, at the bottom of that page, you will find the gate code and the WiFi password.

Spouses / Email / Committees

We have two different ways of emailing members/associates/spouses. We use a mailing program to write to the entire club. This gets around limits of having too many addresses in the To: field. The program sends a separate email to everyone on the list.

Directors however can write to the members of their committees using regular mail. This allows recipients to do a reply all, thus keeping everyone informed on replies. So... if you want a spouse to receive club email, or if a spouse serves on a committee the spouse's email address may have to be entered in two places.

To have a spouse receive club wide email go to your My GYC / My Profile and enter their address in the subscribe form.

To have a spouse serve on a committee, first make sure their name and email address is entered in your profile.

Second, ask Membership to place your spouse on the desired committees.

- Trevor Stokes, Webmaster

How do I reset my password?"

The process is similar to many other web sites you visit.

Desktop: Click on Log In, bottom left

Mobile device: Click on the menu (the hamburger icon, top right) then scroll to find the login link at the end of the menu panel.

1. **Click on** Lost your password?
2. **Enter your username or email address.**
Usernames are typically firstlast with no space. The email address is the one on which you receive club mailings. Click on Get New Password.
3. **Check your email** for a message from GYC with the subject "Password reset."
4. **Click on the long password link** provided in the email, it will take you back to the GYC website.
5. **Create a Password:** Back on the GYC website, you will see a form with a new, impossible password already filled in.
 - If you use a password manager, you can simply save that password.
 - Or, you can enter a password that you can remember. But there are rules for creating a strong password.

***A new password must meet the following specific requirements to reach a required point score:**

- At least 12 characters.
- At least one of each: uppercase letters, lowercase letters, numbers, symbols.
- No character may be repeated 3 or more times. Ordered numeric sequence of more than 2 characters is allowed (i.e. 123 or 321)
- No ordered or patterned sequence of adjacent characters of 4+ alphabetic characters or 3+ numbers (i.e. abcd, abab, 232, 656).

Click on Save Password. If the password is too weak, you will receive an error message. Otherwise, you can now log in.

Launch and Haulout Guidelines

IMPORTANT – Everyone should Read!This!

Fenders, lines, and sling pads must be in place by Friday evening. You must have a **bow** and **TWO stern lines** at least 30' long. Please lead them down so they are accessible from the ground.

Once your boat is launched, each boat captain will check their boat to ensure a dry bilge and that lines are secured, and then move vessel as necessary.

Emergency electric sump pump will be up in the Storage room of the clubhouse in case of emergency. Gas powered pump is in garage.

Launch and Haul Out Committee

- Plans the placement of boats for winter storage
- Arranges for crane services and plans crane schedule

Crane Captain

- Wears different safety vest to allow for easy identification.
- Contacts work crew members to ensure attendance and reviews proper procedures before crew begins tasks.
- Co-ordinates cradle placement and spaces cradles to allow maximum of 4ft. between hulls
- Co-ordinates boat traffic before haul out and after launch.
- Participates in decision making when necessary.

Boat Owner

- Is present at launch or designates an agent and provides information to committee in writing at least a week prior failing which attendance or appointment of an agent by a member, the member is deemed to have given permission to the Haul Out crew to use their best judgement in carrying out the launching of the vessel, and is further deemed to waive any and all claims for any and all damages as may be sustained during the launching.
- Identifies himself/herself to the signal man just prior to his/her boat being lifted.
- Gives instruction to work crew on sling and block placement and gives instruction to signal man when his/her boat is ready for lift.
- Is present for cradle moving one week before haul out and the day after launch – or when otherwise specified.
- Responsible, as always, for their guests (e.g. their safety, where they park, and what they do)
- Boat owner shall ensure that no persons under 18 years of age are present in the work area during launch or haul out

Work Crew

- Act in a safe manner and always be aware of crane activity.
- Stay clear of crane, do not stand under a vessel in slings.
- Must wear safety vests while working on launch crew for easy identification by boat owner and crane operators.
- Fulfill same task during shift. ie lines, slings etc

Signal Man

- Communicates with boat owner and is the only person who gives direction to the crane operator.

Crane Operator

- Makes the final decision about the manner in which a boat will be lifted.

REMEMBER – Do's and Don'ts:

Do have your sling stickers clearly in place

Do have your boat prepared for lift by having: attached bumpers/fenders **one bow and Two stern lines**, that are adequate length to safely handle the boat throughout the entire lift.

Do position your boat at dock before haul out in the same direction it will sit at dry dock to avoid turns while in the slings. (NO turns merely for convenience)

Do be prepared at launch to move your boat once it is in the water to provide room for the next boat, even if you are at your own dock. If your boat is not able to move on its own power, then it is your responsibility to move it as required.

Do help out when extra hands are needed such as moving heavy cradles

- Cradles must be marked with the name of the vessel or the owner.
- Cradles must have skids.
- Cradles must not exceed 36 inches in height for storage purposes.

Don'ts

Reminder: Appendix D of Launch and Haul out. All painting on the hull must be completed 48 hours ahead of launch, to avoid getting paint on the slings and other boats.

Join us on Saturday June 20, 2026 - GYC Sailpast

The History of GYC Sailpast. Check it out at <https://www.georgianyachtclub.com/history-of-sailpast/>

The Tradition of Sailpast

The Sailpast tradition began at the first Yacht Club that was formed at Cowes in England nearly 200 years ago. The membership at Cowes was primarily aristocratic and many were familiar with naval practice and tradition. Cowes Yacht Club subsequently changed its name to the Royal Yacht Squadron and certain social graces were added to the traditional naval practices.

The actual “review of the fleet” is steeped in more than 600 years of Royal Navy tradition and history and was introduced in Yacht clubs as a continuation of the naval habit of having Admirals (and/or Royalty) review the fleet on special occasions.



Protocol demanded that a flagship be anchored with the Admiral and staff on the quarterdeck to receive and return the salute. Vessels sailed past, dipping their colours in salute, and with their captain also saluting with the ship’s company standing at attention. The Vice Admiral would lead the fleet passing in review and the final vessel in the line would carry the Rear Admiral.

Our Yacht Club tradition is almost identical, except that the salute is received by the Commodore instead of the Admiral and the salute is delivered by the passing boat, if under sail, by luffing its jib, or, if under power, by dipping its ensign. All of the crew on the saluting boat stands at attention facing the Commodore, who is the only one to salute by hand. If there is insufficient wind, sailboats should have their engines on.

Protocol dictates that until the Sailpast is completed, only the flagship may be dressed (strung with bunting and flags). However, after the Sailpast, participating yachts are encouraged to dress on returning to their dock.

By precedent, Sailpast is a formal event that officially opens the boating season. The Navy and many yacht clubs also follow the Sailpast with a “Blessing of the Fleet”, a long-standing European tradition, first practiced in Portugal, of seeking divine providence to those who labored or ventured on the sea to assure good harvest, safe passage and safe return. At IYC the Sailpast is accompanied by dressing the clubhouse and flagpole and a formal raising of the national flag, accompanied by a cannon salute. The ceremony is followed by a reception on the lawn.

Like most boating activities, Sailpast is somewhat weather dependent. We all hope for a bright sunny day, calm seas and just enough wind to propel the boats around the course in an orderly fashion. But Mother Nature does not always cooperate. In that case a sailor’s ingenuity comes into play. Tradition dictates that all club members participate in the Sailpast, on their own or on another member’s yacht. Good manners require that a member unable to participate will send his/her regrets to the Commodore.



Contacts for GYC Operations

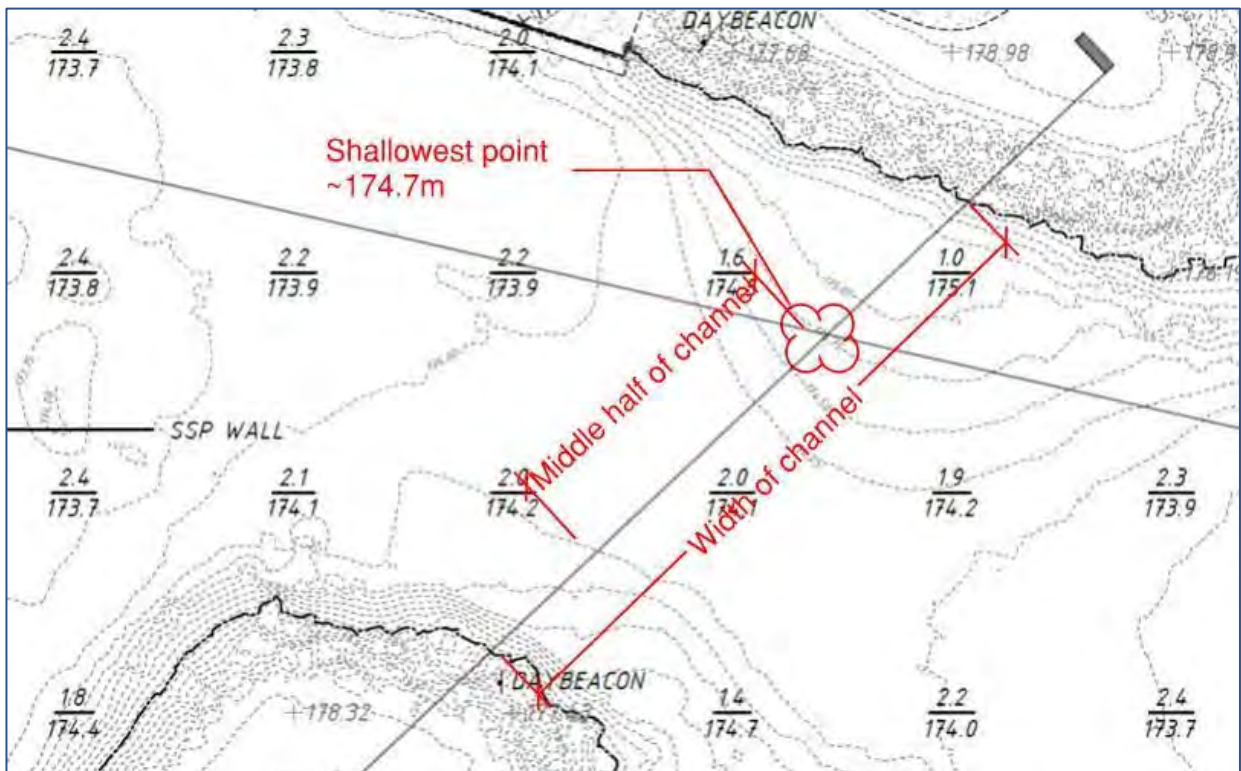
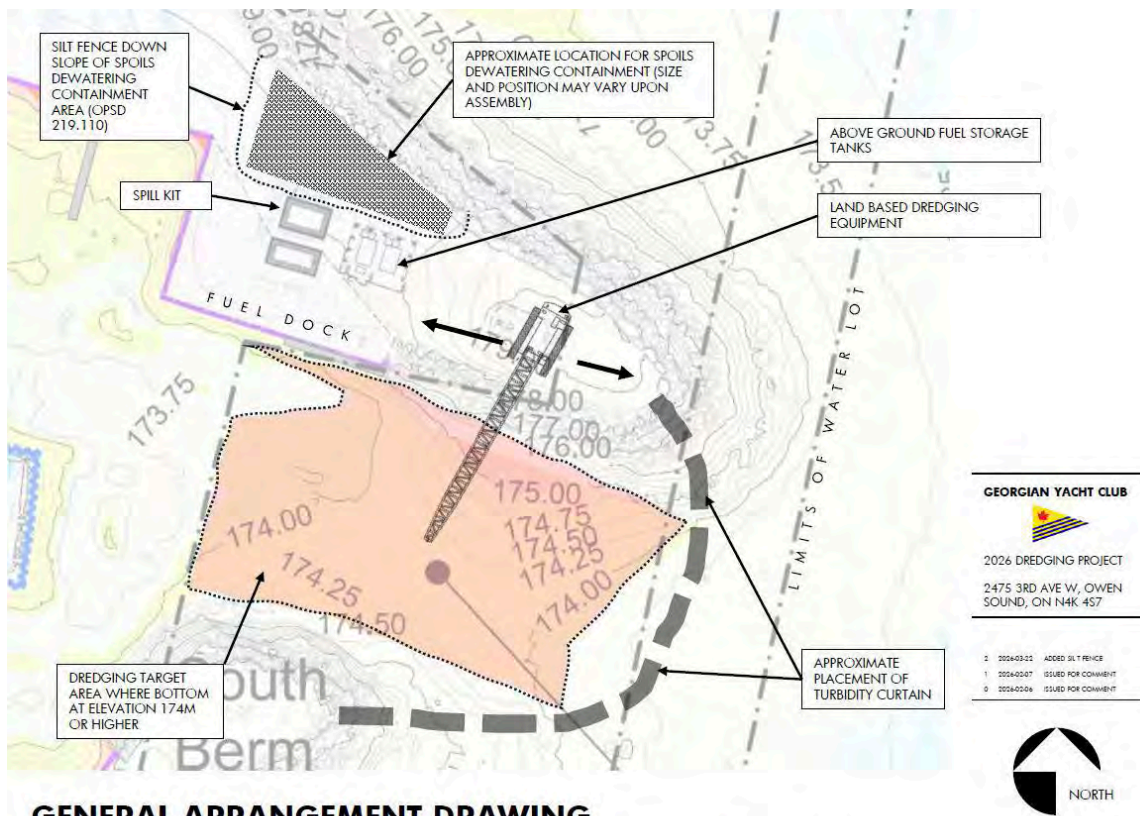
Committee	Name	Phone	Email
Bylaws	Paul F. Middlebrook	519 371-6824 519 372-6920	paulfm33@gmail.com
Commodore	Chris Haslam	226 668-8256	commodore@georgianyachtclub.com
Communications	Scott Caple	647 533-8935	communications@georgianyachtclub.com
Finance	Peter Struthers	519 881 6638(M)	treasurer@georgianyachtclub.com
Fuel Dock and Safety	Murray Arnold	519 378-3833(M)	murrayarnold@hotmail.com
Historian	Sarah Fenton	519 757-9774(M) 226 388-1195(M)	sarahfgyc@gmail.com
Launch and Haul Out	Kurtis Mink	519 387-6465(M) 226 668-2828(M)	kurtismink@gmail.com
Membership	Sarah Fenton	519 757-9774(M) 226 388-1195(M)	membership@georgianyachtclub.com
Mooring	John Griffith	226 755-2956(M)	mooring@georgianyachtclub.com
Permanent Nominating Committee	Becky Middlebrook	519 371-6824(H) 519 379-4992(M)	j24ellamay@gmail.com
Planning and Development	Jason Lehtovaara	519 372-9910 519 386-2685	lehtovaara@bell.net
Privacy Officer	Peter Struthers	519 881 6638(M)	treasurer@georgianyachtclub.com
Properties & Building	Trevor Grasley	519 372-8364	trevorgrasley@yahoo.ca
Sailboat Racing	Paul Weitendorf	519 270-6011(M)	paulweitendorf@outlook.com
Secretary/Vice Commodore	Jason Lehtovaara	519 372-9910	secretary@georgianyachtclub.com
Security	Steve Furness	519 379-3200	Steve.Furness@Grey.ca
Social & Clubhouse	John Gilbert	519 901-2190(M) 519 881-2384(W)	social@georgianyachtclub.com
Technology	Paul King	647 295-7825(M)	paulwkingos@icloud.com
Utilities	Randy Wright	519 372-0810	rwright@bmts.com
White Cloud	John Griffith	226 755-2956(M)	john.griffithmarine@gmail.com

The club also has the following, dedicated email addresses.

Email Address	Notes
boatinsurance@georgianyachtclub.com	Send your boat's insurance certificate to this address. Or give the address to your broker. Most brokers will send the certificate each year when renewed.
bookkeeping@georgianyachtclub.com	Invoices will be sent to you from this address. You will also use this when paying Membership Dues and Fees with an E-transfer payment.
webmaster@georgianyachtclub.com	Use this email address for issues with logging on, problems with the web site , etc.

GYC Calendar

APRIL				
8	WEDNESDAY	7 – 9pm	GYC AGM	
15	WEDNESDAY		Earliest date for boats in the water	
MAY				
2	SATURDAY	8am – 5pm	Launch	
3	SUNDAY	9am – 12pm	Cradle Moving	
4	MONDAY	7 – 9pm	Board of Directors Meeting with New Members	
9	SATURDAY		Spring Cleanup Day	
9	SATURDAY		Fuel Dock Open	
30	SATURDAY		New Member Orientation	
JUNE				
1	MONDAY	7 – 9pm	Board of Directors Meeting with New Members	
7	SUNDAY		Summer Management Team Begins	
20	SATURDAY	1 – 1:30pm	Skippers Meeting SAILPAST	
		1:30 – 2pm	Divine Service Sailpast	
		2 – 3:30pm	Salute Sailpast	
		5:30 – 6:30pm	Dinner Sailpast	
21	SUNDAY		SAILPAST Race	
July				
	29 to Aug 2		Georgian Bay Regatta	
AUGUST				
1-2			Georgian Bay Regatta Continues	
14-15	FRI-SAT		Great Lakes Tugboat Rendezvous	
15	SATURDAY		Commodore Away	
28 to Sept 6			Owen Sound Salmon Spectacular	
SEPTEMBER				
7	MONDAY	7 – 9pm	Board of Directors Meeting with New Members	
18-19	FRI-SAT		GYC Fishing Derby	
24	THURSDAY		Commodore at Home Dinner	
OCTOBER				
17	SATURDAY		Cradle Moving	
23-24	FRI-SAT		Haul Out	
NOVEMBER				
5	MONDAY	7 – 9pm	Board of Directors Meeting with New Members	
15	SUNDAY		All boats out of the water	
31	SATURDAY		MEMBERSHIP YEAR ENDS.	
			Changes to membership & Dock Releases are DUE.	



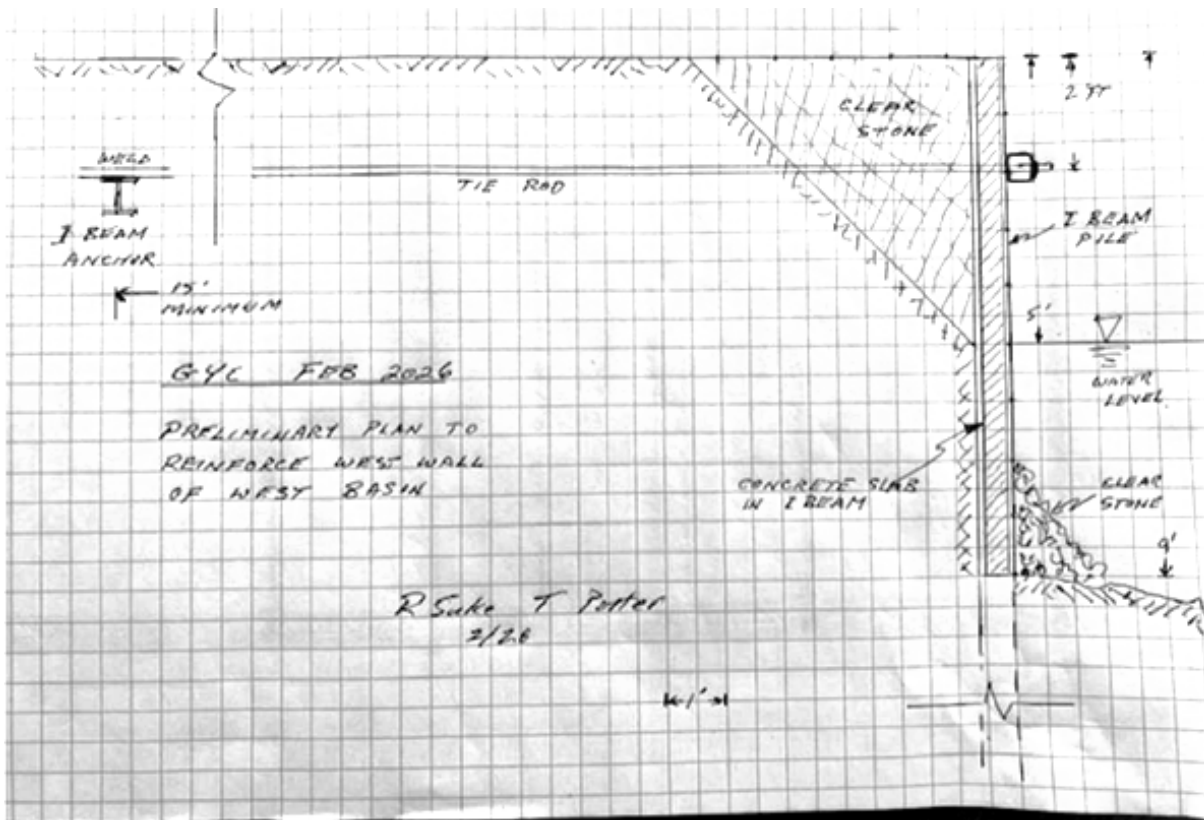
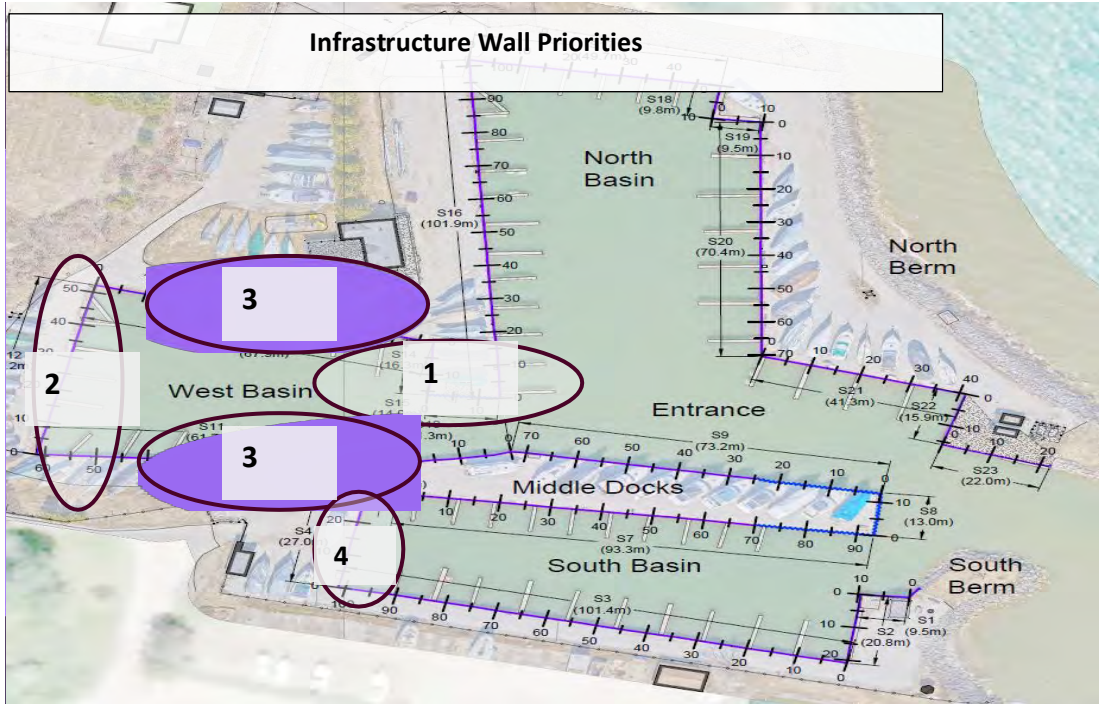


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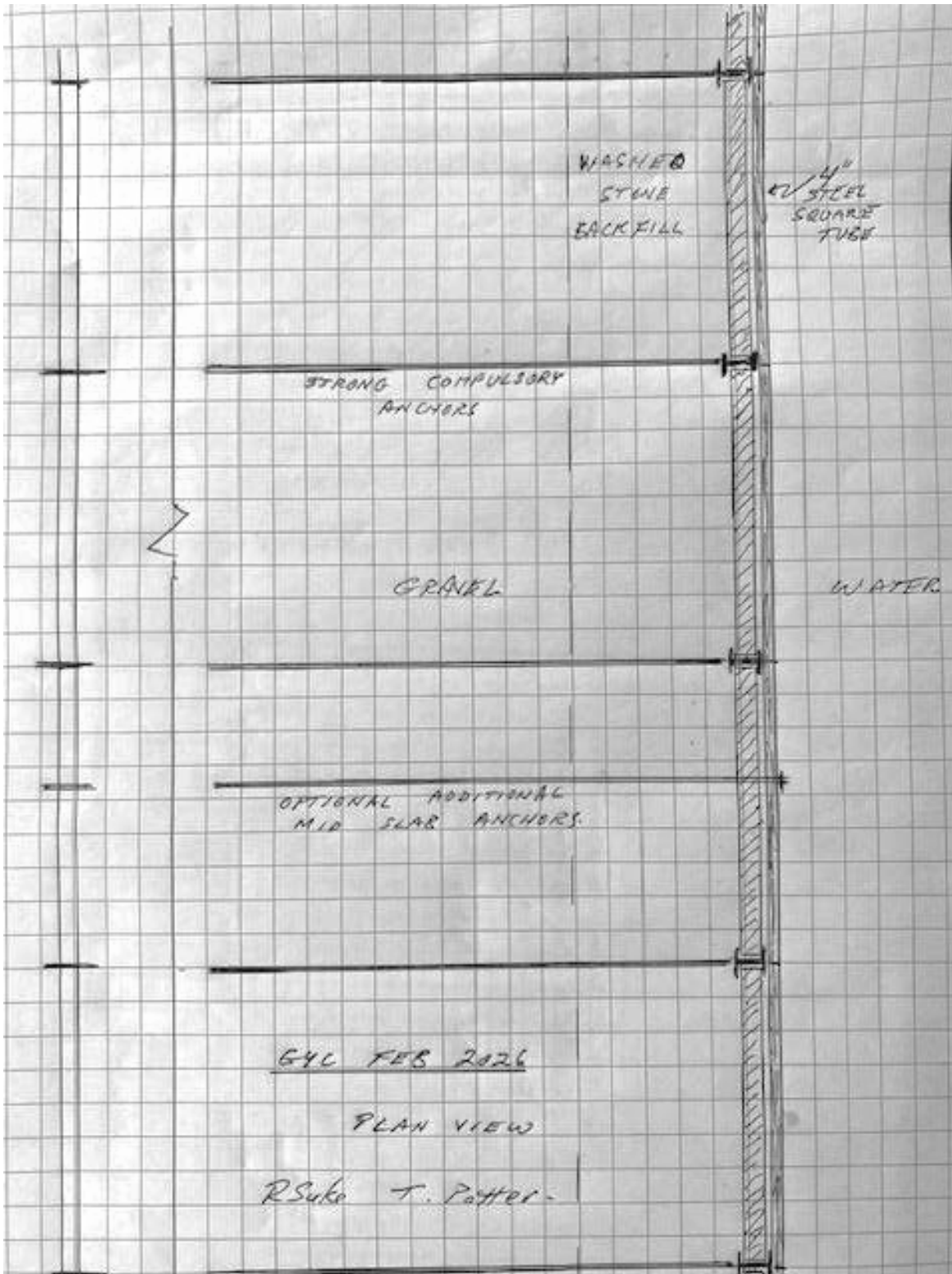


Image credit - Ralph Suke

Last	First	Spouse	Boat Name	Make	Dock	Phone	Email
Adamson	Peter		Piobaireachd	Grampian	R03	519 371-3020	
Allan	Rob	Jan	Island Gypsy		G15	519 374-1585	robllan7@gmail.com
Arnold	Murray	Jan	Monterey	Monterey 206		519 378-3833	murrayarnold@hotmail.com
Arnold	Murray	Jan	Somewhere	Silverton 352	G07	519 378-3833	murrayarnold@hotmail.com
Azad	Sassan	Azar	Caspian	Chapparral	Y13	647 302-3398	sazaq@hotmail.com
BAKER	JOSH	Sandy	Doggy Style	Cadorette Holiday 200	B24	519 373-4838	Bakeshow@me.com
Ball	Ron	Lauretta	Panacea	Mirage	R06	519 371-9812	rwball52@gmail.com
Barfoot	Brent	Ashley	Makin Waves	Power	G12	519 375-4839	brent@wrightgm.com
Barton	Katherine	Andrew	Fidusen	Viking 34	G13	226 923 0981	barton1317@hotmail.com
Beatrice	Paul	Norma	Matthan	Marinette	R34	519 376-0148	paul.norma.b@gmail.com
Beitz	Brad	Lori	Ocha Baby	Sea Ray	Y12	519 378-7450	porky378@hotmail.com
Best	Jeff	Michelle	Playmate	Mach 1	Y01	519 376-3613	michellebest@rogers.com
Beveridge	Garth	Catherine	Anemos	Contest	G03	519-216-3107	garth.beveridge@rogers.com
Blackwell	James	Alison	Booby Bouncer	Yamaha	Y15	519 377-7876	james13blackwell@gmail.com
Boddy	Ian	Joan	Pedlera	C&C	R31	519 373-4144	ianboddy@bellnet.ca
Bonfa	Peter	Doris	SEA YA	Custom Sail		519 371-2824	peterdoris.bonfa@rogers.com
Bonfa	Peter	Doris	Amethyst	Catalina	R30	519 371-2824	peterdoris.bonfa@rogers.com
Bowerman	Steve	Darlene	Chili Pepper	StarCraft	Y08	519 371-2847	loveto38@hotmail.com
Buckley	Joyce		Trivia	Glander Cay	R20	519 794-0739	j.kbuckley@hotmail.com
Buehler	Sue	Duane	Time For Us	Carver	G43	519 580-5414 (Sue's #)	suebuehler59@gmail.com
Burrell	Peter		4 PLAY	Grampian	Y05	519 374-9028	elvisdjkaraoke@hotmail.com
Burrows	Ross	Karin	Regal Affair	Regal 320	B25	519 374-5887	karin.burrows@sympatico.ca
Cagle	Scott	Suza	Jaguar	Honeymoon Sloop	G11	519 370-0760	rscaple@sympatico.ca
Carson	Ryan	Angie	NorthStar	Searay		519 270-9195	rcarson@gmail.com
Chambers	Thomas	Deb	Chambros Two	Newman Trimaran	R01	705 427-3054	chambrostwo@gmail.com
Chester	Mark	Heather	DAZE OFF	Bayliner	B07	519-377-3420	sundaze@rogers.com
Child	Tony	Lois	Childs Play	Carver	B18	226 668 9124	child@brucetelecom.com
Choma	Michael	Anne	Seventh Heaven	Sirius		519 794-4491	chomaanne@gmail.com
Choma	Michael	Anne	Chommarosa	Pursuit	G14	519 794-4491	chomaanne@gmail.com
Collard	Chris	Linda	Windrush	Pearson	R07	519 371-3593	collardchris9@gmail.com
Dawson	Doug	Brenda	Windy	Windy 9800	G17	519 538-2887	doug@boatingwithdawsons.com
Delano	Kevin		Bayliner Discovery	Bayliner	G29	519 379-8887	kevind914@hotmail.com
Doherty	Randall	Louise	JO LEE	Carver	G49	519 372-7112	randydoherty2013@gmail.com
Douglas	Brent	Sophie	Down Time	Rossiter 23	G22	519 372-5572	brent.douglas4@icloud.com
Downie	Ron	Deborah	Down Time	Catalina 320	G37	226 568-2959	rondebdownie@gmail.com
Earnest	Ryan		Kingfisher2	KingFisher	Y06	519 270-8792	earnest_ryan@hotmail.com
Edgar	Doug	Sheila	KITTIWAKE	Jackson		226 668-4454	kittiwake68@gmail.com

Last	First	Spouse	Boat Name	Make	Dock	Phone	Email
Edgar	Doug	Sheila	Brushstroke	Hunter 22	R22	226 668-4454	kittiwake68@gmail.com
England	Tom	Jan	Kalliste	Tanzer	Y18	519 379-8965	tomengland999@gmail.com
Fenton	Sarah	Dave Ellis	Wet Willy	Cruiser Yachts	B01	519 757-9774	sarahfyc@gmail.com
Fenton	Paul	Colleen	Dropzone	Sea Ray	G47	226 664-0733	colleenfenton@hotmail.com
Ferency	Terry	Kim	Clarity	C & C 39	G01	519 377-0663	svclarity@gmail.com
Ferguson	Josh	Amanda	The Dirty Oar	Sea Ray Sundancer 270	B06	519 370-9123	ferg5555@hotmail.com
Ferrari	Nancy	Al	CANAM II	Wilker	G31	519 794-3501	nancy.ferrari01@gmail.com
Findlay	Brad	Vicki	Calypto	Kettle Creek	G56	519 376-0387	findlay101@gmail.com
Forcier	Michael		Warpath	C&C	G45	519 372-5960	mike@greybrucelaw.ca
Ford	John	Valda	Model-T	Legend	Y04	226 664-0960	johnrichardford5@gmail.com
Fryer	Joe	Joanne	Off the Hook	Gilbert Marine	R32	519 371-6621	joefryer47@gmail.com
Furness	Steve	Paulette	Scallywag	Grampian	R10	519 379-3200	Steve.Furness@Grey.ca
Gamble	Janet		Frisky	Kirby30	G19	519 376-1984	janet_gamble@bwdsb.on.ca
Gamble	Bill	Marilyn	Steadfast	C&C 9.9	G02	519 371-3283	marilyngamble05@gmail.com
Gibson	Marion		Second Wind	Mainship	B04	519 371-9234	marionhigbison@outlook.com
Gilbert	John		Come From Away	Aloha	R08	519 901-2190	jwegilbert@gmail.com
Gilbert	John		TIKI MARU	Bayfield 29		519 901-2190	jwegilbert@gmail.com
Grasley	Trevor	Dianne	Georgian Bae	Cruisers	B12	519 372-8364	trevorgrasley@yahoo.ca
Greaves	Frank	Leigh	Blue Sky	MacGregor 26	G32	519 376-4414	F.Greaves@gbicl.ca
Griffith	Ed	Marilyn				519 371-2098	emgriffith@rogers.com
Griffith	John	Courtney Miller	Cedar Bank	Blue Seas	G54	226 755-2956	john.griffithmarine@gmail.com
Griffith	John	Courtney Miller	Scotia Rose	Rosborough RF-246 CWH		226 755-2956	john.griffithmarine@gmail.com
Hachey	Ann	Rudy	Liquid Therapy	Prowler	Y21	519 374-1377	rhachey1224@rogers.com
Hardin	Joseph	Susan	Mujo V	Beneteau	R28	519 534-4197	joseph.hardin@gmail.com
Harmson	Menno	Marjorie	Vrouwe Morgan	Mainship	Y27	226 668-5091	mandmmainship@gmail.com
Harper	Thomas	Shirley	Trophy	Bayliner	G27	705 391-8056	tomharper444@gmail.com
Harrison	[Estate of] Jamie		Northern		G50	519 371-5076	northernharrison@gmail.com
Hartick	Harry	Irene	Drs Orders	Island Gypsy	G57	519 270-0395	hhartick@gmail.com
Haslam	Christopher	Christine	So Hard Done By	Maxum 3500	B05	226 668-8256	treeguy_01@hotmail.com
Hearty	Ken	Penny Lynn	Lasting Memories	Carver 35	G41	519 371-9791	twored71gts@hotmail.com
Howell	Joe	Trisha	Tara 1	Doral	Y11	519 372-0733	jthowell@rogers.com
Hulsebosch	Dave	Pat				519 376-1170	dpbosch@rogers.com
Irvine	Chris	Katie	BIG RED	C&C	R05	519 270-1575	christophergirvine@icloud.com
James	Stephen	Katie McCarthy	Lund	Lund Impact	B19	519 835-2219	stephenjames89@gmail.com
Jarl-Brainard	Birgitta		Bart's Bud	Four Winns	B08	519-372-0659	birgitej2@hotmail.com
Jones	Daniel	Stephanie Jones	Renegade	Abbott 36	R26	519 447-0446	dannjones@live.ca
Jones	Norman	Lynn	Wilfred John	Coaster	R19	519 534-4216	nwjones2020@gmail.com

Last	First	Spouse	Boat Name	Make	Dock	Phone	Email
King	Paul	Susan	Take a Chance	Mirage	R18	647 295-7825	paulwkingos@icloud.com
Krane	Paul	Cheryl Panchuk	Let it Rock	Sea Ray	B15	519 386-6921	paulkrane@gmail.com
Lehmann	John	Linda	Diversion	C&C 30	R15	519 374-3339	sailaweigh1@gmail.com
Lehtovaara	Jason	Sarah	Jigs and Reels	Sea Ray 305 Flybridge	R04	519 372-9910	lehtovaara@bell.net
Livingston	Paul		Simply Complicated	Maxum	B17	519 374-5464	paullivingston55@yahoo.ca
Low	Travis	Michelle	Wet Spot	Cruisers Yacht - 3870	B03	519 376-4088	travis@vandolders.com
MacIntosh	Brian	Rita	Kai-Mac	Prowler	G08	647 298-5300	brianjmacintosh@gmail.com
Mackay	Ewan		CHAOS	Kirby	G33	226 568-4648	ewannackay05@gmail.com
MacKinnon	Brent	Brenda	The Wendy Lee	Rinker Fiesta Vee250	B23	519 477-2559	brentmackinnon9@gmail.com
MacLaughlin	Jim	Elizabeth	Tuppence	Bayfield	R17	519 373-9460	jim_macLaughlin@yahoo.com
Martindill	Roger	Sue	Moonspinner	Grampian	G48	519 794-4537	suemcilroy58@gmail.com
Mathies	Drew	Nancy	Endeavour	Albin	G42	416 427-5477	skiphillfarm@gmail.com
McCannel	Paul	Lori	dontrushme	Chapparel	G52	905 838-0733	paul.mccannel@yahoo.ca
McCartney	Larry	Brenda	Great Expectations		B20	519 371-1551	arooof4u@hotmail.com
McDonald	Kevin	Emilie	Bald Eagle	Striper	Y18a	226 668-9505	wintertextan100@gmail.com
McDonald	Bradey	Shelly		edge water	B10	519 377-3156	bradeym.mcdonald@sympatico.ca
Melay	Jake	Morgan	Start Me Up	Sea Ray 330	B13	519 377-1787	jakemclay17@hotmail.com
McMahon	Robin	Marg	Tip Sea	Hughes	R13	519 274-5149	r.immune@hotmail.com
McMahon	Robin	Marg	Tip Sea Too	Wilker		519 274-5149	r.immune@hotmail.com
McManus	Bruce	Shannon	Offline	Carver	G40	519 379 6861	jbruceememanus@gmail.com
Middlebrook	Paul F.	Becky	NYALA	Bavaria 33	G04	519 371-6824	paulfm33@gmail.com
Middlebrook	Becky	Paul Middlebrook	Ella May	J24	G16	519 371-6824	j24ellamay@gmail.com
Mink	Kimberlee	Adam	Whoaday Hunter	Sea Ray	G51	519 377-3087	Kimberleeminkrmt@gmail.com
Mink	Kurtis	Christine	Why Knot	Egg Harbour	G46	519 387-6465	kurtismink@gmail.com
Mink	Kurtis	Christine	Mink Marine	Grady White		519 387-6465	kurtismink@gmail.com
Mink	Kourtney	Steve Rowen	SeaBreeze	Doral Prestancia	G36	226 974-0765	minker20@hotmail.com
Montgomery	Jonathon		Tidy Time	Power	Y10	519-379-4286	montgomery_454@hotmail.com
Moore	Thomas	Angela	Away From It All	Trophy	B09	519 371-8144	thomasnange@gbtel.ca
Mundle	Brian	Elaine	Arius	Sea Ray	G44	705 429-3363	berundle@yahoo.com
Nicol	Jamie	Martha	Project 50	Blue Seas	G38	519 375-1982	netmarkplus@gmail.com
O'Hara	Chris		Phantom	Phantom	Y02	519-387-2407	c_org@hotmail.com
O'Reilly	Chuck	Sue	Bout Time	Rinker	B16	519 371-3995	coreilly401@rogers.com
Overmars	Gerald	Carol	LE LOUP MARIN I	Elite	G10	226 664-1713	covermars@hotmail.com
Payne	Jason	Linda	Tic Tac Teau	Beneteau First 310	G18	519 376-5950	Jpayne@bmts.com
Poste	Randy		Allegro	Alberg 22	Y17	519 379-5054	randysrecords@wightman.ca
Potter	Tom	Donna	Afternoon Delight II	Regal	G09	519 371-9482	tdpotter@rogers.com
Prentice	Jack		Yachta Know Better	Cruisers	B11	226 974-0409	jackprentice04@gmail.com

Last	First	Spouse	Boat Name	Make	Dock	Phone	Email
Prentice	Kevin			SeaRay	Y20	519 374-1221	kevinprentice69@gmail.com
Prentice	Mark	Darla	Anchor Management	Searay Sundancer 410	Y24	519 376-8100	markprentice@rogers.com
Priester	Jake	Amanda	Ventura	Ontario 28	G35	519 373-6902	jfpriester@hotmail.com
Reitzel	David	Kelly Fenn	NAIDA	Catalina	R09	519 377-0113	d.reitzel@gmail.com
Rissi	Peter		Halcyon	mariner 2+2	R02	226 668-1045	peter.rissi@gmail.com
Rossignol	Lori	Dave	Lark	Niagara	Y16	519 372-8341	lori.rossignol@gmail.com
Rouse	Stephen	Peggy	CARPE' DIEM	Carver 356	G39	519 371-3458	sbrouse99@gmail.com
Russell	Dean	Janeen		Key West	Y14	519 379-4320	russellfencing@live.ca
Schnarr	Jeff	Tammy	Knot flyin	Searay 450	Y23	519 270-2930	jeff.schnarr@sympatico.ca
Scriven	Marija		Tuhf Kook-e	Doral 330 SE		519 377-0858	buffyscriven@hotmail.com
Sewell	Nick	Brooke Sewell	Catch'n Rays	SunDancer 22	B22	705 790-2873	nicksewell114@gmail.com
Shakes	Peter	Shelly	Cocktail Shaker	3300 sundancer	G53	519 375-2540	shellyshakes@bellnet.ca
Shaw	Greg	Shannon	Wingin It	Sea Ray Sundancer 250	B21	519 373 5068	shawzyg@hotmail.com
Simanainen	Vesa	Brenda	GYPSYWIND	C&C	G05	647 922-1833	vsimanainen@gmail.com
Solinger	Frank	Claudia	Solitude	Mirage	R25	519 270-6657	FCSolinger@gmail.com
Sollazzo	Johnny	Stephanie		Proline 190 Sportsman	G30	519 372-4779	johnnysollazzo@hotmail.com
Stokes	Trevor	Emese	MIYotie	Tanzer 22	Y03	519 794-4322	rutlandtrevor@gmail.com
Struthers	Peter	Anne	Liberator	Kirby	R29	519 881 6638	treasurer@georgianyachtclub.com
Suke	Ralph	Marilyn	Mariah	Niagara	R11	519 373-6872	boatsinsurance@georgianyachtclub.com
Sutherland	Harold	Chayle	Ready Mix	Viking	Y19	519 376-4982	chaylesutherland@gmail.com
Sykes	Dave	Cathy		Bayliner	G55	519 372-4749	dvsykes61@gmail.com
Taylor	Creighton	Lois	Sin or Swim	Limestone	G28	519 371-2521	creighton.t@dctaylor.com
Taylor	Susan	Ron	Debutante	Sirius	G20	519 379-2692	susan22692@hotmail.com
van Alphen	Hubert		Blue Jeanne	Tanzer	R23	519 379-3284	hva3284@gmail.com
Van Alphen	Vincent	Alisa	Infinity	Beneteau 323	G06	226 668-8523	vanalphenvincent@gmail.com
Vandestadt	Hubert		SEA BAY AH	Sirius 28	R14	519 376-1688	vandestadt@rogers.com
VanDolder	Lucille		For The Birds	Sea Ray	Y26	519 371-0696	lvandolder@yahoo.ca
Vincent	Mark	Kathy	Quietest Moments	Hunter	R33	519 371-5535	markgrease@yahoo.ca
Wagner	Steve	Kym	Firefly	Wilker	Y18b	519-372-8899	wagzsteve@gmail.com
Waite	Roy	Lorna	Wait & Sea		G21	519 371-0399	rlwaite@gbtel.ca
Walker	Paul	Kathy	Walk on Water	Grady White	Y09	519 373-6707	walker@walkerfinancial.ca
Walton	Adam	Hayley	Mae Lou	Freedom 30	R27	519 270-0206	adamwalton2000@yahoo.com
Warrillow	Ken	Karen	GWENDOLYN III	Bayfield	R16	519 371-1206	k.warrillow@sympatico.ca
Weitendorf	Paul	Shannon	Catch 22	C&C	G34	519 270-6011	paulweitendorf@outlook.com
Wilson	Scott			SeaRay	B14	519 375-4470	scottewilson75@gmail.com
Wright	Randy	Judy	DANAE	Express	R24	519 372-0810	rwright@bmts.com
Zimmerman	Allan		Knotty Buoy	Searay		226 668-8994	tattoos_by_zap@hotmail.com

